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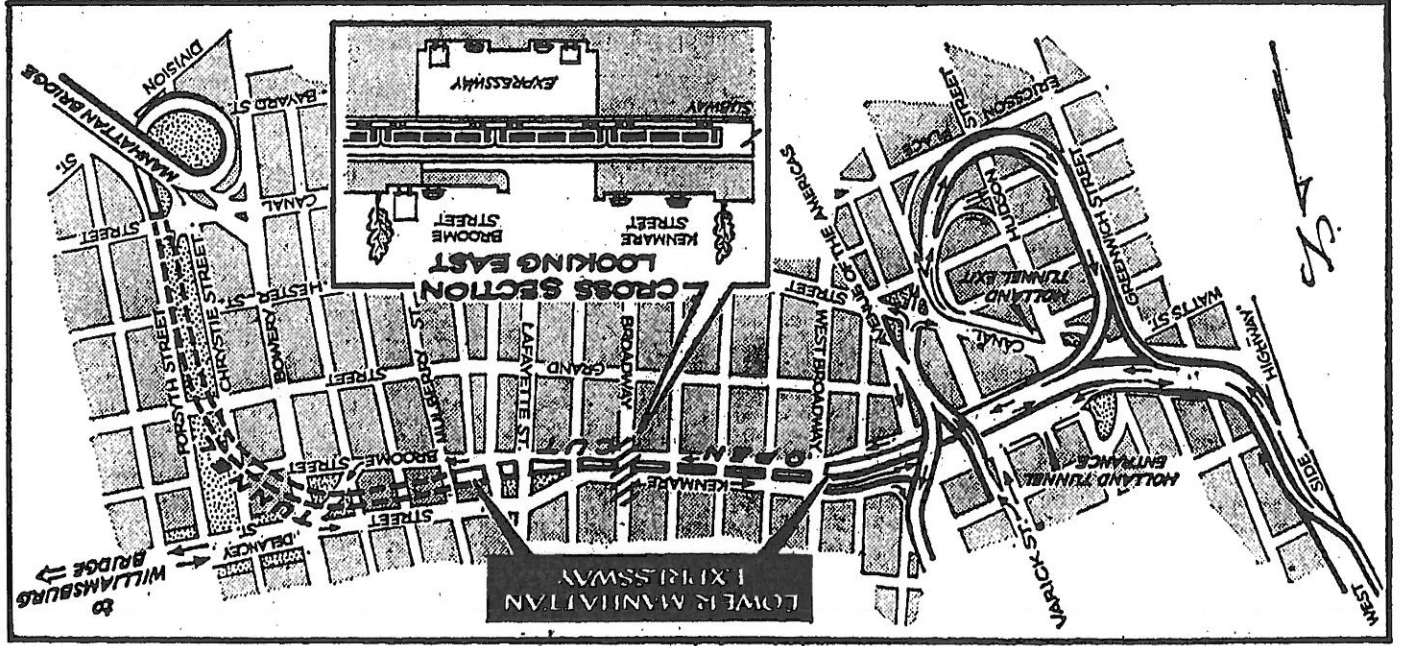
**New Plans Prepared for Downtown Expressway**

By CLAYTON KNOWLES

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pg. 1



**PROPOSED EXPRESSWAY:** Road would traverse Lower Manhattan by surface road, open cut and tunnel

March 28, 1967

**New Plans Prepared for Downtown Expressway**

By CLAYTON KNOWLES

The Lindsay administration has completed engineering plans for a four-lane Lower Manhattan Expressway that would cross the borough in part by tunnel and depressed highway and partly by surface road. The new design for the long-projected and long-debated artery would connect the Manhattan Bridge on the east with the Holland Tunnel

and the West Side Highway. Complementary plans call for developing a six-lane boulevard, three lanes going east and three west, to connect the Holland Tunnel and West Side Highway. The over-all plan has been developed, it is understood, at the direction of Mayor Lindsay to remove objections that led him to scuttle plans for the \$10-million elevated expressway. The new plans, which are

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# New Plans Drawn for Expressway

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Delano Roosevelt Park for 0.25 of a mile. It would swing west across Chrysler Street at Broome Street and continue west, still as a tunnel, 0.21 of a mile to Mulberry Street. From that point, it would proceed 0.44 of a mile as a depressed highway to a point a little west of Avenue of the Americas, where it would rise to street level for along Delancey Street. The present approach to the bridge, into Kenmare Street, which now ends at Lafayette Street, would extend to the Avenue of the Americas. Eastbound motorists would leave the descending traffic circle from the West Side Highway into Broome Street and then travel via Broome Street, extension into Delancey Street. Both east and west roadways of the boulevard are to be 44 feet wide with 16-foot sidewalks on one side and a 3-foot rail on the inner side. The street would cross the depressed and surface portions of the expressway by overpasses. The achieved by a 44-foot cantilevered overhang above the depressed expressway. The route followed in the over-all plan represents basically the same route for the elevated expressway and a later tunnel plan devised by the Regional Plan Association. The tunnel in the new design. The tunnel portion of the expressway would pass 25 feet under the Chrysler Street subway tunnel. Further along, the depressed portion would pass 30 feet under the Williamsburg Bridge. Cost undoubtedly was a major factor influencing the nature of the administration's combination plan, under which the expressway would traverse the borough generally along its just to the north of Broome Street. Getting Federal Aid

Federal authorities, who distributed the 90 per cent Federal assistance for interstate highway programs, have established the policy of not sharing additional costs resulting from local options where a more economical solution is available. Since the new plan reduces the amount of demolition and condemnation that would be required, the chances of getting Federal aid are enhanced. It is felt in official city circles. The new plan will be analyzed by Madigan-Hyland, Inc., consulting engineers on the project for the state. The Long Island City, Queens, concern will examine both its feasibility as to cost and its effectiveness in traffic improvement efforts. Agreement between the city and the state on this course was reached last December. The city, it was learned, has retained Ole Singstad, an authority on tunnels, to analyze the plan thoroughly. Mr. Singstad, who is 84 years old, designed and completed construction of the Holland Tunnel, the Queens-Midtown Tunnel, the Brooklyn-Battery Tunnel, among other projects. He headed the old New York City Tunnel Authority, since merged with the Triborough Bridge and Tunnel Authority. Starting from the Manhattan Bridge traffic circle, already constructed, the expressway would run north under Sarat

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